

HERITAGE ASSESSMENT REPORT

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373

Address: Bordered by Wharf Road, East Terrace, Bridge Street, Railway Terrace, Mannum Road, Hume Reserve Road and the Riverbank: Includes the Murray Bridge and River Murray Rail Bridge, MURRAY BRIDGE, 5253

1. BRIEF HISTORY OF MURRAY BRIDGE TRANSPORT HUB

Murray River Trade & First Bridge

The rise of the Murray River as a trading route began as an entrepreneurial response to the gold-rushes in Victoria. South Australian flour millers discovered that the Murray River was the most efficient route for trade between the gold fields. The Murray River proved to be an important economic artery bringing export products to South Australian ports (wheat and wool) – however the benefits were not immediately felt in the vicinity of the Murray Bridge with most trade occurring between Mannum and Goolwa.

Prior to the construction of the Murray Bridge, the only reliable way to cross the Murray River was via the two punts operated at Wellington. 1864 saw the creation of a parliamentary committee that was tasked with determining the type and best location for a bridge to span the Murray River. The determinations of this committee were longer than expected due to politicians competing to have the bridge built in their electorate. Five designs were requested from engineers in London in 1865 and a year later the bridge was being manufactured with the first sections arriving in South Australia in 1867 without knowing the final destination. By 1872 the bridge site had been selected at a location between Thompson's and Edwards Crossings and the resulting construction would take the next six years. Early in the bridge's construction the 1874 bridge-keeper's cottage was constructed on the western bank overlooking the bridge (Round House, SHP: 13577). As the bridge neared completion an 1878 Bill was passed that authorised the construction of the Adelaide to Melbourne Railway Line – this line would utilise the Murray Bridge and in 1885 a cutting through the cliffs was completed and rails laid upon the bridge to accommodate this. The Adelaide to Melbourne line was formally opened on May 1886.

Development of the Wharf & Railway Station

The railway station was originally going to be located on a 14-acre site at Bridgeport, but it was later decided to locate it closer to the bridge to facilitate the loading and unloading of goods from Paddle Steamers. 1884 saw the surveying of the area that would eventually become the Town of Murray Bridge, known then as Mobilong. In the following year, construction began on the railway complex, and by 1886 it contained wharves, goods shed, and railway station, while the original Bridgeport site was used for engine running sheds, turntables, coal stages, locomotives and goods sidings. This flurry of construction meant that by 1886 the complex included: a second class passenger station, station masters residence, four cottages for locomotive men, running shed, coal stage, 50 foot turntable, weighbridge, two water cranes, 25 000 gallon tank, 6" water main laid to running shed, pumping engine, sixteen foot windmill, home and distant signals, signals and gates, signal cabins on either side of the bridge and refreshment rooms.

Murray Bridge soon became one of the river's major ports with strong trade from nearby Mannum and alterations and extension continued through 1887-8 with additional accommodation and a machine shop for the locomotive department built, while later in 1892 a 30cwt portable steam crane was installed - at the same time alterations were made to the wharf and sidings. 1897 saw the introduction of the first post and telegraph office and structures were relocated from Naracoorte and Mount Barker to be re-erected as the resident engineer's office.

WA Webb & the Second Bridge

Trade on the Murray River could be arguably considered to have begun to decline from 1878 when the government installed a railway line from Morgan to Goolwa and much of the inter-colonial trade had been diverted to rail. However in 1920 the newly appointed Commissioner of Railways, William Alfred Webb authorised an expensive and extensive upgrade of the Murray Bridge Historic Transport Hub that included additional sidings, additions to the station/refreshment rooms and 4 new houses that included a new residence for an electrical fitter.

During this time a new railway bridge was announced in 1923 as the original bridge was found to shudder under the weight of the new, larger trains that Webb had introduced to improve efficiency. This bridge was fabricated by Perry Engineering Co. in Osborne and transported to site. The bridge was completed in 1925 under the supervision of Robert Chapman of NSW Railways and required a 70m tunnel to be cut under Bridge Street.

Beginning of the End

After the construction of the second bridge operations began to be moved to Tailern Bend as it was found that the Murray bridge railway site, nestled between the riverbanks, cliffs and township could not expand to fulfil requirements. 300 staff and the locomotive running sheds were relocated to Tailern Bend, even so Murray Bridge would retain its relative importance as an administrative hub. Despite dwindling traffic the refreshment rooms were remodelled in 1936 and the passenger platform was extended in 1967 and main line re-aligned to accommodate the length of the Melbourne Express. However Murray Bridge never regained its importance and remained little more than an overland stop – the refreshment rooms closed in 1973 and the Murray Bridge that had been reverted to a roadway was superseded by the Swanport [road] Bridge constructed in 1979 that allowed traffic to bypass the town. In 1983 control of the stations signals and points were incorporated into a panel in the Adelaide control centre.

Despite Murray Bridge Historic Transport Hub's short relatively short tenure as a functioning wharf & railway – it has nonetheless been influential in shaping the colony of South Australia.

CHRONOLOGY:

1864	Select committee investigated bridge sites
1868	Ironwork for bridge delivered
1872	Bridge site selected
1873	Construction of bridge began
1874	Round House constructed
1876	River spans of bridge completed
1879	Bridge opened
1885	Bridge adapted to railway use Railway cutting completed Railway opened
1886	Railway Station built
C1892	Loco shed and workshop built
1910	Stone railway cottages built
1906	Station platform extended
1910	Loco barracks built
1915	New railway station built
1923	Concrete railway cottages built
1923	New bridge commenced New railway cutting dug
1925	New railway bridge opened
1926	Railway and loco shed moved to Taillem Bend
1967	Main line re-aligned to accommodate larger trains
1973	Refreshment rooms closed
1979	Swanport Bridge opened
1983	Train control moved to Adelaide

2. DESCRIPTION

The Murray Bridge Historic Transport Hub is a significant precinct that is demonstrative of a C1910's railway complex with adjacent wharf facilities that border onto the Murray River. The site whilst degraded through the removal of several sheds and buildings to Taillem Bend, retains the significant elements of the original site – this includes the two railway bridges that span the Murray River and original bridge master's 'Round House' cottage (1874). Additionally the site includes remnant buildings, plant and elements of the wharf environment with clear linkages to the railway depot site demonstrated through the wharf cutting and sidings that lead to the site of railway operations nearer Hume Reserve.

The original Murray Bridge (1879) lies adjacent to a cutting that links the bridge to the extant railway platform. This line has been superseded by the more modern (c1920)

railway bridge and associated tunnel under Bridge Street. The c1920 railway track also runs west of the bridge to the site of the historic depot site which includes the Edge Moor Iron Co. of Delaware USA turntable as well as the foundations of the sheds that have been removed to Tailem Bend. Also to the west of the bridges, upon the cliff-top remain several workers cottages & amenities that date from c1880 to the time of the South Australian Rail Institute (c1920).

3. ASSESSMENT OF HERITAGE SIGNIFICANCE

Identification of South Australian Historical Themes:

Murray Bridge Historic Transport Hub is important in regards to the way that it contributed to the *social history and development* of the colony of South Australia. Associated with the development of river trade and being an important port for the transport of goods to the Victoria gold fields, Murray Bridge wharf and the Murray Bridge facilitated much of the expansion of the transport and the opening up of South Australia to the eastern states through the overland passenger and goods route to Melbourne.

Murray Bridge Historic Transport Hub is also important in regards to the *economic history* of South Australia – providing an important trade route both through the wharf at Port Mobilong and associated rail to get goods over the Adelaide Hills to Port Adelaide. It was considered common wisdom at the time that expanding rail infrastructure would increase productivity for the fledgling colony. Therefore the South Australian railways were born in a world of optimism that included the building of the first bridge crossing of the River Murray in South Australia.

Murray Bridge Historic Transport Hub is also important in regards to the *political and governmental history* of South Australia. A crossing of the River Murray had long been a goal of the South Australian government and the political process had ensured that a Bridge had been delivered from England long before a location for the crossing had been decided upon. This location of the bridge remained a contentious issue for almost a decade with many members advocating to have the bridge located in their electorate until it was eventually located close to Edwards Crossing. The railways remained a divisive feature in South Australian Politics with many in parliament opposed to the c1915 railway reforms of WA Webb whose efficiency and rationalisation measures are still extant in railway infrastructure today and includes the later rail bridge over the River Murray. The later disinvestment in rail infrastructure that was to follow the removal of WA Webb from leadership of the South Australian Railways was prominent in enabling the decline of rail that resulted in many superseded and underutilised stations finally being abandoned c1980.

Comparability / Rarity / Representation:

Railway complexes are common throughout South Australia. The rise of steam rail (c1880) was associated with multiple gauge tracks and a system where gauge/passenger transfer stations became prominent places. Later developments included the c1915 era in which the control of the railways was changed from a restrictive bureaucratic leadership to a strong centralised system led by WA Webb. Webb was responsible for instituting strong changes, rationalisations and efficiencies all centred on making the railways productive and he included the provisions and upgrades for new larger trains capable of fulfilling this vision. Finally an era of disinvestment in railways, following the ousting of Webb by his political detractors led to the current state of dilapidation of rail infrastructure that was characterised by the

mass abandonment of many railway stations and depots c1980. The rail era of South Australia enjoyed a scant century of operation.

Railway complexes that date from the 1880's, such as Murray Bridge are not uncommon, but vary in levels of listing, significance and significant fabric. Most have a station house or refreshment rooms that are identified at State heritage level whilst elements that relate to physical rail infrastructure (rails, signals) are included in a local heritage place or historic conservation zone; such as Quorn that includes State heritage listed elements such as the railway station building & yard, goods shed, carriage shed and barracks; and Local heritage elements including the power-house & railway bridge that are then encapsulated in the Quorn Historic Conservation Zone.

Several sites include limited significant fabric, due to decommission, dilapidation, removal or other factors such as at Terowie which was the site of a major 'break gauge' passenger and goods transfer station. Terowie included refreshment rooms, extensive sidings, transfer cranes, engine sheds, coal gantry, turntable, weighbridges, reservoir, locomotive barracks and station shade structures. Most of this fabric was reportedly destroyed in a fire in 1967 – but the site retains significance through the remaining State heritage listed refreshment rooms, guard/lamp rooms and lavatories that speak of a much larger complex that would likely include vast archaeological deposits.

Smaller break-gauge stations include Hamley Bridge that dates from 1880 and includes the majority of fabric under the State heritage listing including: Main station building, two water tanks, underground tank, station-masters house, signal cabin, two platforms, platform shelters and crane. Several are smaller sites that include elements such as the sidings and platforms in the State heritage listing due to the existence of little else that defines the station – these examples are more commonly associated with isolated rural examples such as Curdimurka.

Murray Bridge retains only a few items that date from the original c1880 station and depot, including: the rail cutting and the associated Murray Bridge, the original refreshment rooms, turntable and suspected archaeological deposits.

C1910-1920 railway complexes of the WA Webb Era have varying levels of integrity with many having been upgraded and utilised well into the 1970's. Murray Bridge retained administrative functions long after the relocation of sheds and workforce to Taillem Bend – Murray Bridge items that demonstrate this period include the SAR institute, tennis courts, concrete workers cottages and new refreshment rooms that were designed to improve morale for a largely movable workforce. There are no known locations that have been identified as including 'tennis courts' of significance to the State heritage place. Places such as Peterborough include a 'suburb' of concrete railway workers cottages that are not listed. Railway cottages are under-represented on the State heritage register, with only 'station-master' residences appearing regularly as part of a State listing of a complex or appearing individually on a local heritage register. Administrative functions similar to Murray Bridge would have been demonstrated on the Port Augusta railways site as the complex was the headquarters for the Commonwealth trans-continental railway. No fabric was identified from this period of association, certainly not at a level of integrity shown on the Murray Bridge site.

Remaining examples of railway complex have generally been identified on the State heritage register only as components such as the larger turn-tables and roundhouses that are only associated with railways – The State heritage register also includes many examples of items such as water-tanks, goods sheds and signal houses under a larger listing – but items such as rails or tracks, signals etc. are not listed. This is mirrored in the superseded Mile End depot that was designed to increase storage options of the Adelaide railway station c1910 where the site has been largely abandoned and items such as cottages, train shed, small turntable and signal box are not listed in any form. This may be because of the lacking of an associated railway station for listing. The

Murray Bridge Historic Transport Hub nomination seeks to define items such as signals and tracks as significant fabric of the proposed area.

The Murray Bridge Historic Transport Hub is rare in including a wharf that is so strongly associated with the precinct. This wharf and the associated wharf-to-rail infrastructure is of a scale and age that is identical to the remnants at Morgan. The Morgan wharf and rail site was important due to it being the busiest South Australian River Murray port linked to the railways because it allowed goods a more direct route from Sydney to Port Adelaide. State heritage listed elements in Morgan only extend to the wharf structure and the railway station building – elements such as the Morgan wharf spur line and turntable are currently unlisted.

Vegetation is not represented as relating to a State heritage listed railway except in the case of the Oodnadatta stopover station at Coward Springs that includes Date palms and tamarisk trees – the Murray Bridge nomination includes four date palms along East Terrace and Pepper trees next to the 1879 railway cutting that have been identified by the National Trust of South Australia as being of significance to the site.

Murray Bridge Historic Transport Hub includes two fine bridges that have already been State heritage listed and are included in the proposed boundary of the place. These bridges are highly integral to the complex as the town and rail yards were constructed due to their location. No other railway site in South Australia includes bridge crossings of the River Murray – these crossings are additionally integral to the significance of the proposed precinct.

Murray Bridge Historic Transport Hub remains the greatest rail and river interface in the history of South Australia and despite the removal of several sheds still retains a high integrity that development has not yet obscured. A report issued by Austral Archaeology and Peter Bell is the first archaeological assessment of a railways site on the state heritage register and potential archaeological deposits have been identified. The current protections of the site as a Historic Conservation Zone under the *Development Act 1993* do not adequately protect these areas identified as having archaeological deposits of state heritage significance. The report also identifies a significant boundary for the site designed to protect identified significant fabric that encompasses land beyond the railway reserve and this boundary has informed the proposed boundary of the proposed listing - other listed railways complexes limit their significance to the gazetted land parcels and do not take into account the potential cultural artefacts that such sites retain.

Assessment against Criteria (Under Section 16 of the *Heritage Places Act 1993*):

(a) it demonstrates important aspects of the evolution or pattern of the state's history.

In considering this criterion, I have had regard to the provided Guidelines for State Heritage Places that note:

The place should be closely associated with events, developments or cultural phases which have played a significant part in South Australian history. Ideally it should demonstrate those associations in its fabric.

Places will not normally be considered under this criterion if they are of a class of things that are commonplace, or frequently replicated across the State, places associated with events of interest only to a small number of people, places associated with developments of little significance, or places only reputed to have been the scene of an event which has left no trace or which lacks substantial evidence.

Murray Bridge Historic Transport Hub demonstrates important aspects of the evolution and pattern of railway transport and the interaction of the colony of South Australia with the River Murray. The precinct is associated with the beginnings of River Murray trade as Edwards and Thompson's Crossings were frequented by river-boats in the beginning wool and wheat trade with the colony of Victoria. Despite being the first South Australian crossing of the river, the Murray Bridge (1879) had been constructed at the beginning of the decline of River Murray trade and the important linkages between the river and rail are demonstrated on site in the forms of the wharf, wharf spur line and original railway cutting that led to the 1879 Murray Bridge.

The Murray Bridge Historic Transport Hub was a highly important stop on the overland rail route to Melbourne, with Refreshment rooms that catered for passengers having just undertaken the long trip over the Adelaide hills. The precinct is associated with the rise of steam railways from c1880 and includes many elements from this time including the rail cuttings, original station and turntable, however the major improvements to the precinct were overseen by WA Webb as part of his c1920 scheme to make the railways more efficient and productive. Under this scheme major administrative functions were retained at Murray Bridge and upgrades included new refreshment rooms, workers and engineers cottages, SAR institute and tennis courts – all designed to improve morale of the largely relocatable workforce.

This scheme of improvements under WA Webb were not localised nor special to Murray Bridge with most railway depots/stations throughout South Australia being targeted by Webb's upgrades and rationalisations. Murray Bridge Railway Precinct was however, favoured with the later Rail Bridge designed to be able to support the larger trains integral to Webb's vision - this was concurrent with additional rationalisations where due to a lack of space in the Murray Bridge Depot, the depot was moved to Tailem Bend along with rail sheds and 300 relocated workers.

Despite the removal of items to Tailem Bend, Murray Bridge Historic Transport Hub is highly demonstrative both in absence as well as presence, of the vast range of evolution throughout South Australian of the South Australian Railways and Murray Bridge's early beginnings as a junction with the River Murray paddle-steamer trade.

Murray Bridge Historic Transport Hub fulfils this criterion.

(b) it has rare, uncommon or endangered qualities that are of cultural significance.

In considering this criterion, I have had regard to the provided Guidelines for State Heritage Places, that note:

The place should demonstrate a way of life, social custom, industrial process or land use which is no longer practised, is in danger of being lost, or is of exceptional interest. This encompasses both places which were always rare, and places which have become scarce through subsequent loss or destruction.

Places will not normally be considered under this criterion if their rarity is merely local, or if they appear rare only because research has not been done elsewhere, or if their distinguishing characteristics have been degraded or compromised, or if they are at present common and simply believed to be in danger of becoming rare in future.

Railway precincts such as Murray Bridge are not rare in South Australia - most railway depots include or have included many elements similar to the Murray Bridge Historic Transport Hub. However Murray Bridge is of rarity due to being one of only two examples

of railway complexes that are significant due to the intersection with the River Murray, the second is located at Morgan. Morgan includes much of the same wharf-to-rail infrastructure as the Murray Bridge nomination, however Murray Bridge further distinguishes itself through being the only site with bridge crossings of the River Murray that are integral to the heritage significance of the place.

Most significant railway complexes have become significant due to their intersection of competing broad and narrow gauge rails creating a complex that was required for the changing of trains by passengers – Murray Bridge’s significance was instead due to the intersection with the river and remains the only large scale WA Webb Era depot with a River Murray connection. However, many of the elements that relate to the transport depot that were expanded and upgraded by WA Webb’s c1925 rationalisations are not rare and are frequently replicated, this includes items associated with the depot, workers cottages, SAR institute. The Murray Bridge Refreshment Rooms are visually identical to stations at Tailem Bend, Penola, Bordertown, Wallaroo, Moonta and Victor Harbour.

Despite the several items being replicated throughout railway heritage, the Murray Bridge Historic Transport Hub fulfils this criterion.

(c) it may yield information that will contribute to an understanding of the state's history, including its natural history.

In considering this criterion, I have had regard to the provided Guidelines for State Heritage Places, that note:

The place should provide, or demonstrate a likelihood of providing, information that will contribute significantly to our knowledge of the past. The information should be inherent in the fabric of the place. The place may be a standing structure, an archaeological deposit or a geological site.

Places will not normally be considered under this criterion simply because they are believed to contain archaeological or palaeontological deposits. There must be good reasons to suppose the site is of value for research, and that useful information will emerge. A place that will yield the same information as many other places, or information that could be obtained as readily from documentary sources, may not be eligible.

Murray Bridge Historic Transport Hub has long been associated with rail and river trade, and can demonstrate continuous occupation from the 1870’s. As expected of a site of this nature and size, there are likely to be archaeological desposits of State Heritage significance.

Austral Archaeology & Peter Bell have identified sites within the boundary of the Murray Bridge Historic Transport Hub as having significant archaeological deposits that are likely of State heritage significance to South Australia. Most often they relate to places where buildings or structures have been demolished or removed but there are others that are of technological significance that are wholly or partially intact and/or may benefit from archaeological investigation. These places vary from sites of minor utility buildings through to major buildings and human-made features such as the original cutting and the turntable. These include:

- The site of the removed locomotive workshops
Given the heavy duty nature of this industrial building with probable below ground elements (ash pits etc) it is likely that the remains can provide detailed information about the structure, its operation and mode of removal. There may well be some associated cultural deposits.
- Site of removed Old Cottage

This is an early Murray Bridge building and sources of information about it are likely to be scant. May well be extensive associated cultural deposits.

- Site of removed Depot and Motor Sheds
Not much is known about this complex from historical sources. May well be some associated cultural deposits.
- Site of removed Cubicles
Investigation may provide more information than currently available.
- Pumping Station footings and earthworks
Not much is known about this feature from historical sources at the present time. May well be some associated cultural deposits in the form of tools and equipment. It could benefit from clearance and exposure.
- Site of removed goods shed
Site may be able to provide new information about construction and detailing of the shed floor and column bases etc.
- Sites of other removed buildings in wharf precinct
Little is known about site, however information may be available from other sources.

Austral Archaeology & Peter Bell have additionally identified that further archaeological deposits are likely to be found; firstly in the vicinity of the reserve adjacent to the wharf that relate to the historic (1870's) wharf sheds and and secondly in vicinity of the later River Murray Shipping Company sheds & slipway.

Murray Bridge Historic Transport Hub is the only railway depot and complex that has been assessed for archaeological deposits. Current Local Heritage Place (LHP 14218) protections under the *Development Act 1993* do not adequately protect archaeological deposits. Therefore for the rarity of being a railway heritage site with identified archaeological value, Murray Bridge Historic Transport Hub fulfils this criteria.

(d) it is an outstanding representative of a particular class of places of cultural significance.

In considering this criterion, I have had regard to the provided Guidelines for State Heritage Places, that note:

The place should be capable of providing understanding of the category of places which it represents. It should be typical of a wider range of such places, and in a good state of integrity, that is, still faithfully presenting its historical message.

Places will not be considered simply because they are members of a class, they must be both notable examples and well-preserved. Places will be excluded if their characteristics do not clearly typify the class, or if they were very like many other places, or if their representative qualities had been degraded or lost. However, places will not be excluded from the Register merely because other similar places are included.

Murray Bridge Historic Transport Hub is difficult to identify as specific class of place as it fulfils the criteria to be considered as wharf, railway station and railway depot – all of these classes can be considered significant in the context of South Australia.

Murray Bridge Historic Transport Hub does not include an outstanding representative of a wharf – Murray Bridge's wharf and siding have been significantly altered with the laying of bitumen and the dilapidation of important elements including the crane, rails & adjacent crane shed. A finer example of a River Murray wharf can be found at Morgan where conservation works and the associated conservation management plan have retained significant fabric.

Murray Bridge Historic Transport Hub is not an outstanding representative of a railway station - of immediate comparison is the 'Pichi Richi' railway at Quorn whose extant elements produce a place of high integrity. The Murray Bridge Historic Transport Hub station house and associated buildings are common to railway stations of c1915. The design of the Station/Refreshment rooms is visually identical to stations at Tailem Bend, Penola, Bordertown, Wallaroo, Moonta and Victor Harbour. Other buildings are of a style frequently replicated including concrete workers cottages that are better demonstrated by a 'suburb' of Local heritage listed buildings at Peterborough. Associated platforms, sidings and other railway ephemera are common to railway complexes are rarely included in a State heritage listing except when the railway station is of a size that requires the identification of all such elements- these are usually listed as including a platform or siding such as at Curdimurka, whose complex is defined by the Stuart Creek Bridge and associated siding. Additional comparison can be made with Hamley Bridge whose platforms have not been so extensively upgraded as Murray Bridge and have retained original 1880's fabric.

When Murray Bridge Historic Transport Hub is compared against the State heritage listed railway depots, it can be considered that sites such as Terowie (1881) that were integral in supplying the servicemen in the North for the First World War and Port Augusta (1881) built as part of the first section of the 'Great Northern Railway' were far greater complexes and despite little remaining fabric would be considered as being of higher significance than the Murray Bridge nomination.

Despite Murray Bridge Historic Transport Hub's long occupancy, most extant significant fabric is demonstrated as being associated with the rationalisation works of WA Webb and must be compared with later railway depots such as Tailem Bend (1914) that became significant under the rationalisation works of WA Webb and include greater amounts of demonstrable significant fabric. Tailem Bend includes State heritage listed elements such as a significant turntable and Railway station building, unlisted elements include: goods platform, crane, goods/train sheds, water tower, Round house and set of scales. Port Augusta also demonstrates this style of listing with a listed Station House and unlisted seemingly extant elements that may include carriage sheds, concrete tanks, workers cottages, stores, 45ft turntable, sidings, superintendent house, pump house and fuel tanks that date from c1920.

Murray Bridge Historic Transport Hub demonstrates fabric that is rare to railway depots, that include significant earthworks including cuttings and tunnels and dual bridges that are specifically related to the siting and choosing of the site as a rail depot. Whilst places such as, but not exclusively Port Augusta, Curdimurka and Quorn are associated with rail bridges, no depot site is so topographically defined by their connection to their bridge.

However, when Murray Bridge Historic Transport Hub is considered as a wharf/rail interface with continuous occupation from 1874 it remains in a class of only two examples, the other being Morgan. Due to the significant crossings of the River Murray and the association with a significant depot, Murray Bridge must be considered as the most outstanding example of this interface.

Therefore Murray Bridge Historic Transport Hub fulfils this criterion.

- (e) it demonstrates a high degree of creative, aesthetic or technical accomplishment or is an outstanding representative of particular construction techniques or design characteristics.**

In considering this criterion, I have had regard to the provided Guidelines for State Heritage Places, that note:

The place should show qualities of innovation or departure, beauty or formal design, or represent a new achievement of its time. Breakthroughs in technology or new developments in design would qualify, if the place clearly shows them. A high standard of design skill and originality is expected.

Places would not normally be considered under this criterion if their degree of achievement could not be demonstrated, or where their integrity was diminished so that the achievement, while documented, was no longer apparent in the place, or simply because they were the work of a designer who demonstrated innovation elsewhere.

The Murray Bridge Transport Precinct includes elements that may be considered as having high technical and aesthetic accomplishment. Of technical accomplishment are the dual rail bridges that in both their span and construction are highly significant. The 1879 State heritage listed Murray Bridge is a Warren girder bridge and is considered the 'oldest and most important metal truss building in South Australia' (O'Connor cited in Donovan, 1992). The 1925 State heritage listed Rail Bridge is considered a significant engineering structure with two-parallel chord Pratt trusses of up to 56.4m and a curved upper chord with a span of 73.2m.

Additional technical works include the c1880 rail-cuttings and wharf spur line that through location and depth demonstrate the technical accomplishment and construction techniques of the early builders in providing access for the trains to their respective destinations on the wharf and on the bridge. Other elements that make up both the wharf and rail depot, whilst technically significant are common.

Of significant aesthetic accomplishment is the 1874 State heritage listed Round House. The Round House was State heritage listed in 1987 and the 1994 *Round House Conservation Policy* states that:

'the building is of an unusual design and very high quality in its use of materials, construction detail, and workmanship. The original part of the house is a very refined mid-Victorian structure, with an unusually shaped and roofed verandah/balcony surround, and unusual asymmetrical form. It has touches of opulence in the use of materials and decoration, rarely seen in such remote sites so far from the City of Adelaide at that time.'

Bruce Harry & Associates, 1994, p30

However, when considered in the context of other buildings in the state, and in the light of current criteria as part of this Assessment Report of the whole Murray Bridge Historic Transport Hub, the site is not considered to meet this criterion to a high degree, and therefore does not fulfil this criterion.

- (f) it has strong cultural or spiritual associations for the community or a group within it.**

In considering this criterion, I have had regard to the provided Guidelines for State Heritage Places, that note:

The place should be one which the community or a significant cultural group have held in high regard for an extended period. This must be much stronger than people's normal attachment to their surroundings. The association may in some instances be in folklore rather than in reality.

Places will not be considered if their associations are commonplace by nature, or of recent origin, or recognised only by a small number of people, or not held very strongly, or held by a group not widely recognised, or cannot be demonstrated satisfactorily to others.

Despite some associations with the local community of Murray Bridge and history-group RailSA, there is no strong representation at a State heritage level. Murray Bridge Historic Transport Hub does not fulfil this criteria.

(g) it has a special association with the life or work of a person or organisation or an event of historical importance.

In considering this criterion, I have had regard to the provided Guidelines for State Heritage Places, that note:

The place must have a close association with a person or group which played a significant part in past events, and that association should be demonstrated in the fabric of the place. The product of a creative person, or the workplace of a person whose contribution was in industry, would be more closely associated with the person's work than would his or her home. Most people are associated with many places in their lifetime, and it must be demonstrated why one place is more significant than others.

Places will not generally be considered under this criterion if they have only a brief, incidental or distant association, or if they are associated with persons or groups of little significance, or if they are associated with an event which has left no trace, or if a similar association could be claimed for many places, or if the association cannot be demonstrated. Generally the home or the grave of a notable person will not be entered in the Register unless it has some distinctive attribute, or there is no other physical evidence of the person's life or career in existence.

The Murray Bridge Historic Railway Precinct is not associated greatly with the life and work of any person or organisation. The period of choosing the location of the Murray Bridge is significant in of itself, however no player in the South Australian Parliament at the time is directly credited with championing its final destination.

The opening of the first bridge crossing in South Australia of the River Murray was an event of historical importance, however this has already been taken into account in the previous State heritage-listing of The Murray Bridge.

WA Webb and the reforms that were undertaken to make the railways more efficient were part of a much larger vision for the SA Railways that included many station and depot upgrades – Murray Bridge was one of many of these upgrades and despite the attention paid that included a new rail bridge over the Murray, this attention was due in part of functional requirements rather than the Webb and the SAR having particular association with this depot and station.

Therefore Murray Bridge Historic Transport Hub does not fulfil this criterion.

Extent of Listing / Significant Fabric / Curtilage:

The extent of listing includes:

An area bordered by Wharf Road, East Terrace, Bridge Street, Railway Terrace, Mannum Road, Hume Reserve Road and the Southern-most Riverbank: Includes the Murray Bridge and River Murray Rail Bridge.

The components identified as having **high significance** in the context of the Place are defined on Site Plan 2 and summarised below.

1. Murray Bridge Road Bridge (SHP: 10287)
2. Murray Bridge Railway Bridge [Metal Truss] (SHP: 13826)
3. Murray Bridge Wharf & Hand Crane (SHP: 13828)
4. Murray Bridge Railway Station & Refreshment Rooms (SHP: 13820)
5. Round House (former Murray Bridge Works Superintendent House) (SHP: 13577)
6. Edge Moor Iron Co. of Delaware USA turntable 1883
7. Railway spur line adjacent to riverbank
8. Water Column
9. Original railway cutting including broad gauge tracks
10. Four (4) stone railway cottages 1910
11. Three (3) concrete railway cottages
12. Timber signalman's cottage c1883 including annexes
13. Timber railway office buildings including annexes
14. Station Masters (Foreman's) residence
15. River Murray Shipping Company Shed and Slipway
16. Rail Tunnel under Bridge Street
17. Archaeological sites identified of having high or moderate chance of discovering deposits of high to moderate heritage significance by Austral Archaeology & Peter Bell including:
 - a. The site of the removed locomotive workshops
 - b. Site of removed Old Cottage
 - c. Site of removed Depot and Motor Sheds
 - d. Site of removed Cubicles
 - e. Pumping Station footings and earthworks (*No. 20 on site plan: 2*)
 - f. Site of removed goods shed
 - g. Sites of other removed buildings in wharf precinct

The components identified as having **medium significance** in the context of the Place are defined on Site Plan 2 and summarised below.

18. Timber South Australian Railways Institute
19. Stone railway (loco) barracks 1910
20. Pumping Station Footing & Earthworks
21. Former goods yard, town wharf including remaining walls and infrastructure
22. Loading ramp
23. South Australian Railways tennis courts
24. Pillbox

The components identified as having **low significance** in the context of the Place are defined on Site Plan 2 and summarised below.

25. Centralised Train Control building 1967
26. Historic Advertising Hoarding Boards
27. Access bridge over original cutting Broad gauge tracks and layout
28. Fences constructed of rails (*Fence between 12 & 13 on site plan 2*)
29. Tomato shed
30. Remnant native vegetation and regeneration
31. Historic exotic vegetation
32. Row of four (4) Canary Island Palm trees on cnr East Terrace/Bridge Street

31. Pepper Trees adjacent to the 1879 cutting

The extent of listing excludes:

Any items that are not associated with the railways & rail travel, River Murray shipping, the wharf or considered a significant archaeological site.

REFERENCES:

Austral Archaeology & Bell, P 2011, *Archaeological Assessment of the Murray Bridge Historic Transport Hub*, Prepared for the Heritage Policy Unit (HPU), Department of Environment and Natural Resources (DENR)

Bruce Harry & Associates 1994, *Round House Conservation Policy*, Prepared for the Rural Council of Murray Bridge

Donovan & Associates 1992, *Railway Heritage of South Australia*, Report commissioned by the National Trust of South Australia (NTSA)

Laurence, S 2009, *Murray Bridge Railway Precinct*, Heritage Branch File Note, File No: 13949A/26286

National Trust of South Australia 2011, *Murray Bridge Historic Transport Hub*, Nomination for a South Australian Heritage Place

RailSA 2012, *Murray Bridge Railway Precinct*, Nomination for a South Australian Heritage Place

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373

SITE RECORD:

FORMER NAME:

Murray Bridge Railway Reserve Complex, Murray Bridge Railway Precinct

DESCRIPTION OF PLACE:

The Murray Bridge Historic Transport Hub is a significant landscape that is demonstrative of a C1900's railway complex with adjacent wharf facilities that border onto the Murray River. The site whilst degraded through the removal of several sheds and buildings to Tailern Bend, retains the significant elements of the original site – this includes the two railway bridges that span the Murray River and original bridge master's 'Round House' cottage (1874). Additionally the site includes remnant buildings, plant and elements of the wharf environment demonstrating clear linkages to the railway depot site such as the wharf cutting and sidings that lead to the site of railway operations nearer Hume Reserve.

The original Murray Bridge (1879) includes a cutting that links the bridge to the extant railway platform. This line has been superseded by the modern (1925) railway bridge and associated tunnel under Bridge Street. The modern railway track also runs west of the bridge to the site of the historic depot site which includes the Edge Moor Iron Co. of Delaware USA turntable as well as the foundations of the sheds that have reportedly been removed to Tailern Bend. Also to the west of the bridges, upon the cliff-top remain several workers cottages & amenities that date from c1880 to the time of the South Australian Rail Institute (c1920).

It is noted that the integrity of the site includes the views to the river from the workers cottages and round house and wharf cutting connections under the bridges to the wharf crane and elements of Sturt Reserve.

DATE OF COMPLETION:

1925

REGISTER STATUS:

Description:

The proposed boundary encompasses the five (5) State Heritage Places listed below:

- Murray Bridge Road Bridge
Date Registered: 28 May 1980
- Murray Bridge Railway Bridge
[Metal Truss]
Date Registered: 29 Jun 1989
- Murray Bridge Wharf & Hand Crane
Date Registered: 17 Dec 1987
- Murray Bridge Railway Station & Refreshment Rooms

Date Registered: 23 Jun 1994
 - Round House (former Murray
 Bridge Works Superintendent
 House)
 Date Registered: 28 May 1987

The majority of the bounded
 area is a Historic Conservation
 Zone in the Rural Council of
 Murray Bridge Development
 Plan.

DATE: For registered dates see above.

CURRENT USE: **Description:** Railway Reserve, Public Reserve, Train Station
Dates: 1983-Current

PREVIOUS USE(S): **Description:** Transport Hub
Dates: 1879-1967

ARCHITECT: **Name:** N/A
Dates: N/A

BUILDER: **Name:** Various
Dates: From C1879

LOCAL GOVERNMENT AREA: **Description:** Rural Council of Murray Bridge

LOCATION: The Area bounded by the riverbank, Wharf Road to Railway Terrace and Hume Reserve Road, Murray Bridge, South Australia, 5253 and includes the two (2) adjacent state heritage listed bridges, and three (3) state heritage places within this boundary – see site plan for boundary details

OWNER(S) & LAND DESCRIPTION:

Area: Murray Bridge
Hundred: Mobilong

[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]

Type: CR Vol: 5684 Folio: 861	Type: CT Vol: 5808 Folio: 957	Type: CT Vol: 6086 Folio: 666	Type: CT Vol: 6040 Folio: 94	Type: CT Vol: 5222 Folio: 299
Type: CT Vol: 5844 Folio: 386	Type: CT Vol: 5419 Folio: 291	Type: CT Vol: 5931 Folio: 785	Type: CT Vol: 6040 Folio: 95	
Type: CR Vol: 5754 Folio: 295	Type: CR Vol: 5759 Folio: 651	Type: CT Vol: 5222 Folio: 300		
Type: CR Vol: 6015 Folio: 16	Type: CT Vol: 5222 Folio: 301			



SITE PLAN 1: Murray Bridge Historic Transport Hub (Extent of Listing)

Bordered by Wharf Road, East Terrace, Bridge Street, Railway Terrace, Mannum Road, Hume Reserve Road and the Riverbank: Includes the Murray Bridge and River Murray Rail Bridge, MURRAY BRIDGE, 5253

— Extent of nomination

— State heritage places

Scale: 1:5000 N↑



SITE PLAN 2: Murray Bridge Historic Transport Hub (Component Locations)

Bordered by Wharf Road, East Terrace, Bridge Street, Railway Terrace, Mannum Road, Hume Reserve Road and the Riverbank: Includes the Murray Bridge and River Murray Rail Bridge, MURRAY BRIDGE, 5253

Scale: 1:5000 N↑

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
The Murray Bridge (SHP: 10287)



Murray Bridge Historic Transport Hub
Murray Bridge Railway Bridge [Metal Truss] (SHP: 13826)

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Murray Bridge Wharf & Hand Crane (SHP: 13828)



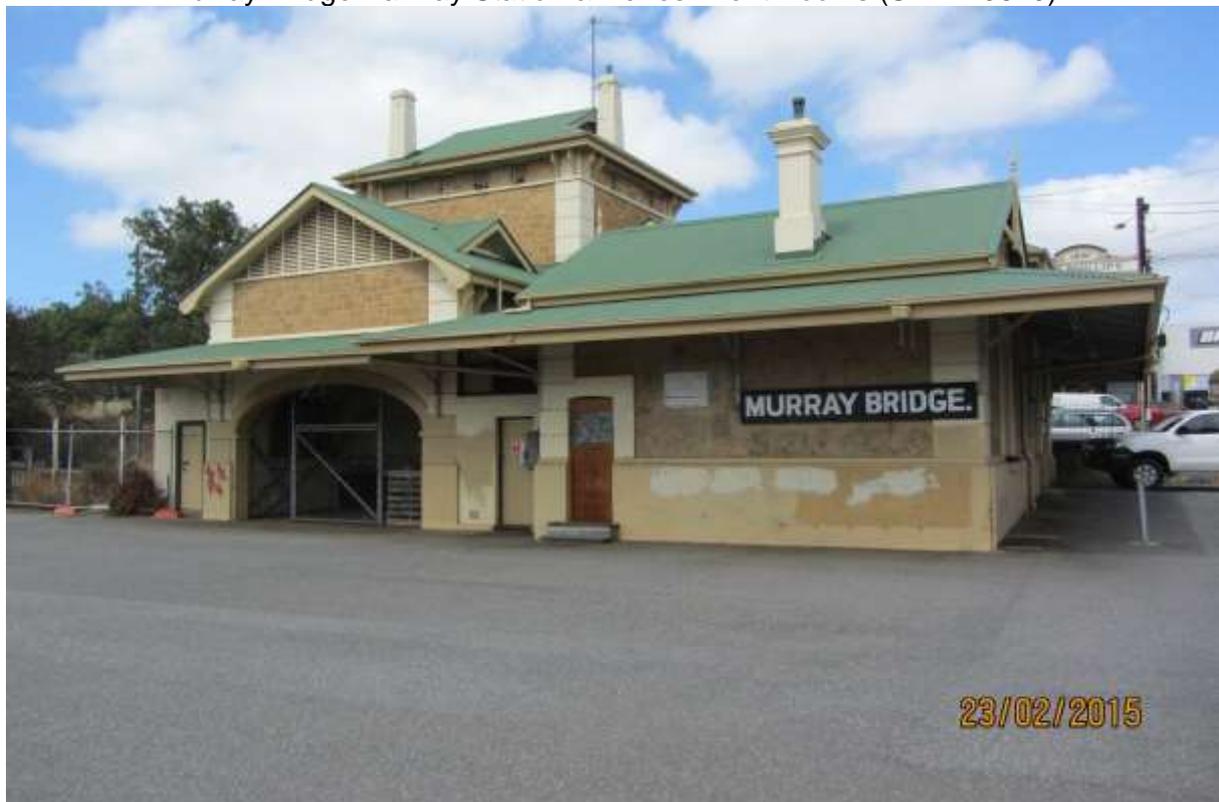
Murray Bridge Historic Transport Hub
Murray Bridge Wharf & Hand Crane (SHP: 13828)

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Murray Bridge Railway Station & Refreshment Rooms (SHP: 13820)



Murray Bridge Historic Transport Hub
Murray Bridge Railway Station & Refreshment Rooms (SHP: 13820)

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Round House (former Murray Bridge Works Superintendent House) (SHP: 13577)



Murray Bridge Historic Transport Hub
Edge Moor Iron Co. of Delaware USA turntable

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Timber signalman's cottage c1883 including annexes



Murray Bridge Historic Transport Hub
Centralised Train Control building 1967

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
South Australian Railways tennis courts with fences made of rails



Murray Bridge Historic Transport Hub
Stone railway (loco) barracks 1910

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Station Masters (Foreman's) residence



Murray Bridge Historic Transport Hub
Stone railway cottage 1910

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Concrete railway cottage



Murray Bridge Historic Transport Hub
Original railway cutting including broad gauge tracks and access-bridge

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Railway spur line adjacent to riverbank



Murray Bridge Historic Transport Hub
Former goods yard, town wharf including remaining walls and infrastructure

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Former goods yard, town wharf including remaining walls and infrastructure



Murray Bridge Historic Transport Hub
River Murray Shipping Company Shed and Slipway

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Pumping Station Footing & Earthworks



Murray Bridge Historic Transport Hub
Water Column (standpipe)

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Pillbox (telephone box)



Murray Bridge Historic Transport Hub
Loading ramp

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Tomato shed



Murray Bridge Historic Transport Hub
Row of Canary Island Palm trees

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Depot archaeological site



Murray Bridge Historic Transport Hub
Depot archaeological site

NAME: Murray Bridge Historic Transport Hub

PLACE NO.: 26373



Murray Bridge Historic Transport Hub
Foundations adjacent to Depot archaeological site



Murray Bridge Historic Transport Hub
Sites of other removed buildings in wharf precinct (Sturt Reserve)